

Interlochen Development District Conceptual Enhancement Plan



2009

Interlochen Development District
Enhancement Plan
2009



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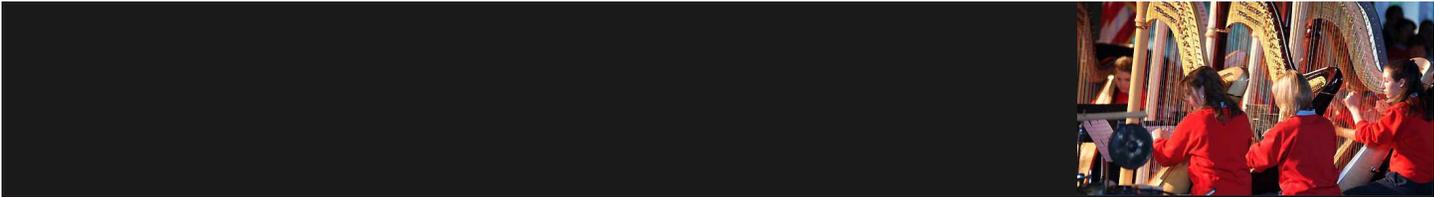
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INTRODUCTION

Introduction

The Interlochen Development District (IDD) is in many ways defined by its surrounding environment. The land and water played a critical role in settlement of the area, its development and economy. The area's natural features are equally important today and are the district's greatest asset. Other valuable resources are the district's pleasant rural atmosphere, woodland and lake views, plentiful green space, seasonal events, and rich arts heritage. Building upon the positive, there are also characteristics that could be improved. The small-scale, village-oriented retail and service district is low density and stretched along a 1-mile corridor. This low density, 1-mile distance, and lack of sidewalks or pathways make walking within the district difficult. Wide roadway shoulders currently serve as pedestrian pathways but can be uncomfortable to use due to proximity and speed of traffic. Parking in the central part of the district is informal and often bleeds into adjacent lawns and roadways. Land uses at the US-31 and M-137 intersection are large-scale and vehicular oriented. These land uses are similar to others along the US-31 corridor from Traverse City and do not currently indicate the intersection as a gateway to Interlochen.

This physical assessment considers many factors that contribute to the functionality and aesthetic of the IDD. These include Interlochen's history, different character zones within the district, approaches and entrances to the district, vehicular circulation and parking, pedestrian circulation, and character defining features that provide IDD its unique identity.

History

Before the Village, there was the land and water. It fostered settlement with its towering timber, clean water and plentiful wildlife. The area's primary timber industry spawned other industries such as railroads, barrel manufacturing and services that supported the logging camps. Until the early 1900's, Interlochen life followed this pattern and the land's primary role was one of necessity and sustenance. In the early 1900's, the railroad, and later the automobile, made recreational travel much simpler. As a result, the area began a transition to a recreation destination. Camp Pennington, Interlochen State Park and numerous lake cottages trace their origins to this period of development.

The transition to recreation took another leap forward in 1928 when the National High School Orchestra Camp (later known as Interlochen Center for the Arts) established itself between the lakes. Over time, the area became a summer destination and began to take on its present "up north" feel. Over the next several decades Interlochen's economy and character became firmly established as a recreation and arts oriented community. The type of recreation has changed, but that heritage remains today, as does physical evidence of the community's past.

Character Zones

The first impression of the Interlochen Development District is of a small, relaxed and comfortable village. A closer study begins to reveal some of the district's rich history, but also some realities that detract from one's pleasant first impressions. In order to better make suggestions of how to emphasize what is positive and improve what is not, the district corridors have been divided into 6 character zones. While each zone contributes to the overall look and feel of the district as a whole, each has a distinct character of its own.

Interlochen Gateway

This is the point of entry for most visitors into the district and consists of the US-31 corridor and its intersection with M-137. Of the 6 character zones, Interlochen Gateway is most removed from the village's historic and cultural assets. Land uses in the zone include retail, commercial, and significant surface parking lots. There is also a significant amount of undeveloped property.

The gateway zone's architecture is a contemporary blend typically found in suburban thoroughfare districts. This includes variations on box stores, multi-tenant buildings and auto service facilities. In addition to the typical, Fun Country adds some uniqueness to the otherwise ordinary architecture mix.

While the gateway zone is the primary visitor entrance to the Interlochen area, it does not have a character that matches its importance. Currently, small highway signs for Interlochen State Park and Interlochen Center for the Arts are the only indications that something special can be found south on M-137. Approach and entrance to the district will be discussed later in this assessment.

The gateway zone is vehicular oriented and is not easily walkable. Roadways are high speed and heavy volume. Parking lots are large and lack landscape treatments. Very limited pedestrian sidewalks or pathways exist.

Village Retail Corridor

The Village Retail Corridor zone is located along M-137 between 1st Street and Riley Road. Land uses are a low density mix of retail, commercial, residential and office space. Architecture is an eclectic blend of small-scale development. Many buildings are converted from their original purpose for a second use. Building setbacks vary. Some properties have green lawn frontage some do not. Parking is informal and often lacks defined edges. Most parking is not landscaped or screened. Vehicular access is directly from M-137 through individual access points. Pedestrians currently utilize the wide shoulders on M-137.

Historic Rail Center

The Historic Rail Center is located at the historic crossing of the Pere Marquette and Manistee & Northeastern railroads. This is also where a railroad depot and intersection tower once stood. Today, the vacated east/west rail bed is owned by several different entities and is enjoyed by equestrian riders and users of off road vehicles and snowmobiles. The north/south rail bed is now used as East Railroad Avenue. Land uses adjacent to the Historic Rail Center include low density retail and residential.

Historic Village Center

The Historic Village Center zone is located at the intersection of M-137 and Riley Road. It is the location where a village hall and hotel once stood. Land uses currently include retail, commercial, and institutional. Due to the creek and alignment of M-137, the Historic Village Center serves as a unique threshold for entering and exiting the village. It is also bears the closest physical proximity to Interlochen State Park and Interlochen Center for the Arts.

Greenway Corridor

This is the wooded portion of M-137 between the Historic Village Center and Arts & Culture Center. Land use is largely undeveloped woodland with limited residential. Ownership adjacent to M-137 is by Interlochen Center for the Arts or Interlochen State Park. The corridor is a pleasant transition between the Village and Arts & Culture Center. Pedestrians use 8' wide M-137 shoulders as a pathway between the two destinations although traffic proximity and speeds can make the walk uncomfortable.



Historic Rail Depot



Early Settlement Lumber Industry

Arts & Culture Center

This zone consists of Interlochen State Park and Interlochen Center for the Arts (ICA). Entrances for both are within close proximity along M-137 and include associated signage. Both facilities maintain a wooded edge on M-137. The only development visible from the road is a series of gravel parking lots for ICA. The State Park and ICA are connected by a pedestrian pathway and crossing of M-137.

Approaching and Entering the Interlochen District

There are four major approaches to the Interlochen Development District; US-31/M-137 intersection from the north, Karlin Road from the south, Riley Road from the west, and the abandoned rail bed from the east and west. Each offers a unique experience and first impression.

US-31/M-137 Intersection from the North

This is the primary vehicular entrance to the Interlochen area for most visitors. It is the route taken by those traveling from Traverse City, Long Lake, Frankfort, Empire, Leland, and other points north. It is also the entrance most removed from the Village's unique character. Users approach from east or west US-31 by crossing low marsh areas and coming uphill to the M-137 intersection. Land uses in the vicinity of the intersection are typical suburban large-scale retail, commercial and automobile service. Retail and commercial signage is dominant while wayfinding signage is limited and not quickly distinguishable from other roadside items. As a result, Interlochen does not have a presence at the intersection and visitors are not intuitively guided to turn south onto M-137. Once heading south on M-137, the first ¼-mile is dominated by the large scale land uses before crossing 1st Street and entering the Village Retail Corridor that contributes greatly to Interlochen's rich character. Pedestrian access from this approach does not currently exist.



US-31 and M-137 Intersection

Karlin Road from the South

This is largely an entrance to the Interlochen area for local traffic. Those traveling from Karlin, Buckley, Thompsonville, Copermish and other points south enter from Karlin Road. Users drive north on two-lane Karlin Road through largely wooded areas. Occasional breaks in the woodlands occur at residences and locations such as the Fireplace Inn, Green Lake Township Airport, and Memorial Park. Continuing north, users arrive at the entrances to Interlochen Center for the Arts and Interlochen State Park.

At this point, Karlin Road changes designation to become M-137. Both entrances are well marked with signage, landscape and a series of banner poles on both sides of M-137. From there, users continue north on M-137 through the wooded Greenway Corridor described previously. Just before entering the Village, M-137 curves and crosses a creek. When passing through the curve, the village quickly appears and makes for a rather dramatic entrance. The first portions of the village visible when rounding the curve are Interlochen Elementary School and Bud's restaurant. Users coming from Diamond Park Road join M-137 and also have this same entry experience.

Riley Road from the West

This is primarily a local route from the near west side of Interlochen and Green Lake Township. Riley Road is a two-lane rural blacktop and is largely wooded on both sides with occasional breaks for residences. When entering the village from Riley Road, users pass the Interlochen Public Library and low density residential before arriving at the Historic Village Center at the intersection of Riley Road and M-137. There are currently not pedestrian facilities along the Riley Road corridor.

Abandoned Rail Bed from the East and West

The rail bed entrance to the village is used by equestrian riders, off road vehicles, snowmobiles, mountain bikers, and pedestrians. When approaching from the east or west, the rail bed is surrounded by woods and then dramatically opens up when it enters the village. Within the village, the rail bed passes by residential, retail and commercial land uses. Public use of the rail bed terminates approximately 1-mile east of the village at Tonawanda Road. The rail bed continues beyond Tonawanda Road but is privately owned. West of Interlochen, users follow the rail bed for several miles and can make informal connection via county roads to DNR ORV Trail 39 in Benzie County. Of all approaches to the district, this one is perhaps the most underdeveloped and at the same time has the most potential to give visitors a strong sense of the community's heritage and character. Several organizations are working to obtain easements that would allow for public access for the full length of the rail bed. This would provide formal trail connections to DNR trails in Benzie County and Traverse City.



Abandoned Railway Corridor through the Historic Village area



Interlochen Center for the Arts street / corridor banners

Circulation and Parking

Directional and Identification Signage

Currently, little public directional and identification signage exists. What does exist is visually overwhelmed by private identification signage, especially within the US-31 Corridor. Existing private identification signage for Interlochen Center for the Arts may serve as a starting point for an overall signage program which identifies the district as a whole, public properties and other significant destinations within the district. Directional signage is also a primary component to any signage program. Particularly important are signs and other design clues that lead drivers to intuitively turn south onto M-137 from US-31. Other, secondary signs may direct drivers to public parking.

Vehicular Circulation and Parking

The two primary roads in the district are US-31 and M-137. Both are two-lane rural highways with large, paved shoulders and are regulated and maintained by the Michigan Department of Transportation. Access points to adjacent properties are mostly informal gravel drives or parking lots. Some formalized, curbed, and paved access points are found in the vicinity of the US-31/M-137 intersection. Like the access points, most parking in the district is in the form of informal gravel lots. Most lots lack definition or screening and bleed into adjacent lawn areas. Parking within the Interlochen Gateway zone is in the form of large, paved lots with little screening or interior green space.

Non-motorized Circulation

In general, the Interlochen Development District does not have specific provisions for non-motorized traffic. Some of the newer developments near the US-31/M-137 intersection do include sidewalks though development scale and traffic volumes make non-motorized transportation very unpleasant. In other portions of the district, low traffic volumes and small-scale, rural character make walking and biking along the wide road shoulders possible though somewhat uncomfortable. Another challenge to non-motorized travel is the district's size and low density development. It is approximately 1-mile from US-31 to Riley Road and 1-mile from Riley Road to the Interlochen State Park entrance. Most visitors will walk within about a ¼-mile radius of where they park. This reality means that within the Village Retail Corridor most visitors will not be inclined to stroll from shop to shop without relocating their car. It also means that most visitors will not be inclined to make the 1-mile walk between the village and Arts & Culture Center. However, these are still vital non-motorized links for students and recreational visitors.

Perhaps the greatest non-motorized asset currently existing in the district is the DNR's rail bed. Current trail users include ORVs, equestrian riders, mountain bikers and pedestrians.

Character Defining Features

“Up North” Feel

As stated earlier, the area’s natural features are clearly the district’s most visible and valuable assets. The land and water fueled the early economy, spawned settlement and initiated development of the early summer camps. It is also the environment’s natural beauty that draws visitors to enjoy today’s recreational opportunities. The eclectic architecture and retail mix also contribute to the village’s relaxed, rural feel.

Arts & Culture

Interlochen Center for the Arts (ICA) and Interlochen State Park (ISP) share a unique relationship with each other and with the village. Both are destination points and draw many visitors to the area. In addition, many residents are linked philosophically and/or financially to ICA and ISP. Art and history are unmistakably important to the Interlochen community but are currently not physically represented in a way consistent with their importance. In other words, a visitor to the village may not quickly understand how important art and history are to the community.

Landscape, Lighting, Paving and Street Furniture

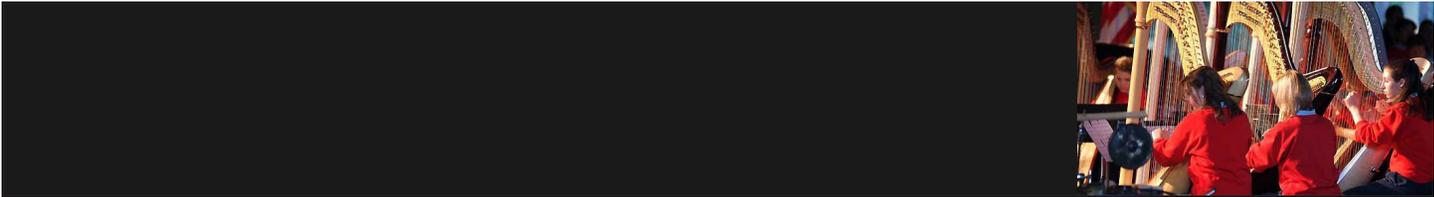
The most significant landscapes exist natively and in the residential portions of the village. Many areas of the district are undeveloped and heavily wooded. In residential areas, existing mature trees and generous front lawns contribute to the relaxed, rural feel of the area. Newer developments within the Interlochen Gateway and Historic Village Center zones have generous landscape although some lack sufficient parking lot landscape to visually reduce the expanses of parking pavement. Older developments in all zones lack landscape and parking lot screening.

Lighting in the Interlochen Development District is sporadic in type and location. Intersections along US-31 and M-137 have overhead “cobra” style fixtures directly over the intersections without lighting the center of the blocks. If lighted, off-street parking lots in the district use a variety of post-top fixtures ranging from unshielded floodlights and “shoebox” fixtures to more appropriate variations on “inverted dog dish” style fixtures that contribute positively to the area’s relaxed, rural character.

Paving in the district is rather consistent in material and design. Public streets are bituminous when paved or well maintained gravel when unpaved. All streets, except for the US-31/M-137 intersection are not curbed and have drainage swales within green shoulders. Most off-street

parking lots in the village are gravel although a few are bituminous without curb and gutter. Most parking lots in the Interlochen Gateway zone are bituminous without curb and gutter. Sidewalks where present are scored concrete and are in good condition.

Like lighting fixtures, street furniture in the district is varied. Various businesses have outdoor seating utilizing a broad cross section of furniture ranging from wood benches to metal tables and chairs. Business identification signage varies in design but maintains a fairly consistent character through the district. Most are appropriately sized and designed to positively contribute to the village’s “up north” character.



Interlochen Development Authority

June, 2009

Planning for Streetscape Design Underway

Streetscape Update

The Interlochen Development Authority (IDA) has contracted with Beckett & Raeder, Inc. (BRI) to produce a Streetscape Design Plan for the Interlochen area. The project corridor is roughly defined by M-137 between US-31 and the Interlochen Center for the Arts entrance including US-31 adjacent to its intersection with M-137. BRI presented its initial physical assessment at an IDA meeting on May 11, 2009, (please see page 2). The physical assessment resulted from BRI's review of existing township planning documents, proposed development plans, and historical surveys. The project team also spent significant time in the community observing and documenting existing conditions.

The physical assessment identified many positive characteristics existing in the community today. Valuable resources such as woodlands, lake views, seasonal events and the area's rich arts heritage define the community's mindset and deserve to be celebrated. In addition to the community's wonderful rural and rustic feel, visitors are drawn by the area's abundant recreational and educational opportunities. One of the greatest underutilized potentials is the DNR owned railbed and the recreational and economic benefits it could provide. Potential is also offered by several distinct character zones within the M-137 corridor that are identified in the accompanying graphic. These character zones will be described more in future newsletters. Greater than all

of these physical assets is the commitment and passion held by the area's business owners and citizens.

Challenges within the area are largely access related, both vehicular and non-motorized. The US-31/M-137 intersection is rather non-descript and does not intuitively inform visitors of the special community and places south on M-137. Once on M-137, development is at a very low density which is partially responsible for the area's great rural character. However, the distances between buildings make walking the length of the village more than most visitors care to do. Visitors and residents alike tend to feel uncomfortable w

BRI will be develop address these ch on the positive ; the area's rural a

For More Info

Call: Krysti E Zoning (231) 2
email: krysti@

Streetscape Update



Interlochen Development Authority

September, 2009

Streetscape Design Planning Continues

Since printing of the last newsletter, the Interlochen Development Authority (IDA) with assistance from Beckett & Raeder, Inc. (BRI) has continued development of a Streetscape Design Plan for the Interlochen area. The project corridor is roughly defined as M-137 between US-31 and the Interlochen Center for the Arts entrance including M-137 and US-31 intersection. BRI presented design plan concepts at an IDA meeting on June 8, 2009 and additional concept refinements at a meeting on August 10, 2009. The concepts evolved from and responded to observations made in the physical assessment summarized in the last newsletter.

The concepts seek to maintain and enhance many positive characteristics existing in the community today including resources such as woodlands, lake views, seasonal events, the area's rich arts heritage and its wonderful rural and rustic feel. The concepts also address challenges identified in the physical assessment such as non-motorized access, lack of a community gateway at the US-31 intersection, and overall length of the corridor. Some of the challenges identified are larger issues than those typically addressed as part of a streetscape design. As a result, the plan includes a conceptual master plan for the corridor, associated land use zoning possibilities and design guideline recommendations.

During the June and August meetings, BRI presented specific design concepts for highlight areas within the corridor. Currently, the US-31/M-137 intersection is rather non-descript and does not intuitively inform visitors of the special community and places south on M-137. BRI presented a "Community Gateway" concept that includes non-motorized improvements, colorful plantings, and a field of large banners to provide a significant entry to the community. Two other highlight areas are the Historic Rail Center and Historic Village Center. Concepts were pre-

sented of these two Centers that propose concentrating future development in the areas. Currently, very low density development on M-137 makes walking from business to business more than most visitors care to do. Focusing future development in the two Center areas will create a more walkable downtown environment to better support activities and businesses within the community.

Next steps in the project will be to compile drawings and recommendations into a report format for use by the community in seeking grant opportunities.

Open House Come see the Streetscape Design Plan!

**Come and go open house.
Hors d'oeuvres provided.**

**4:00-7:00pm
September 21, 2009**

**Interlochen
Public Library
9700 Riley Road**

For More Information:

Call: Krysti Baker
Zoning Administrator
(231) 276-9329

email: krysti@greenlaketownship.org



PUBLIC OUTREACH

For a bit of history, the Interlochen Development Authority (IDA) was formed in 2007 with the knowledge that regional planning efforts identified Interlochen as a potential growth center in part because of its recreational and cultural assets. The IDA has also recognized that the character of the community's major corridor (M-137) does not physically reflect its recreational and cultural values. To that end, the IDA commissioned development of this Streetscape Design Plan. The IDA's Steering Committee (SC) worked tirelessly with the project team throughout the course of the project, directing them to sources of background information, reviewing assumptions and providing feedback at important project milestones. The broad perspectives of SC members provided a management tool for keeping the project on target and ensuring that the project team always received direction. Members of the SC have a passion for the community and their time and insight was invaluable to the process. Members of the SC include Bob Allard, Krysti Baker, Barry Bialik, Paul Biondo, Sarah Bourgeois, Narda Hughson, Pat McDonald, Dave Pennington, Brian Postma, and Leslie Sickterman.

The SC members also deserve a special note of thanks for their open-mindedness and flexibility during the project process. Although this project initiated as a "streetscape" project, it quickly became evident to the SC and consultants that the corridor could also benefit from some larger picture planning. As a result, the project has evolved into a unique blend between a master plan and a streetscape plan in order to best serve the needs of the community. This would not have been possible without the SC members' foresight.

Project Process

The Interlochen Development Authority retained Beckett & Raeder, Inc. to facilitate the planning process. The project area defined by the IDA consists roughly of the M-137 corridor starting at and including the US-31 intersection on the north and extending south to the Interlochen Center for the Arts. The project process followed a logical sequence of tasks as outlined below:

Timeframe	Action
February 2009	Kick-off Meeting
May 2009	Initial Consultant Site Visit and Informal Interviews
May 2009	Presentation of Initial Physical Assessment to Steering Committee
June 2009	Presentation of Initial Design Concepts to Steering Committee
July 2009	Presentation of Refined Design Concepts to Steering Committee
September 2009	Steering Committee Meeting and Community Open House
October 2009	Final Draft Report to Steering

Several study efforts and planning documents preceded the Streetscape Design Plan. In 2007, the Interlochen Village Area Plan was adopted. Within the document are a detailed series of goals and objectives that provided valuable guidance for this current streetscape study. Also adopted in 2007, the Interlochen Improvement District Development & Tax Increment Financing Plan lists potential projects within the area and their associated timeframes. Michigan State University's Small Town Design Initiative Program studied the IDA district in 2008. The student project was titled Interlochen: Image/Identity/Sense of Place and resulted in a variety of concepts that opened the community's collective mind to new ideas and possibilities.



Walk-In Open House held during September 2009

The Outreach and Engagement Strategy

The successful outcome of the planning process relied on the active participation of the community. The public outreach process established a solid foundation for enlisting the community support and interest, and the shared commitment needed for development of the Interlochen Streetscape Plan. Information compiled for the Interlochen Village Plan (11-2007) was also consulted to determine the community's vision for the future corridor.

Early in the project process, informal interviews and conversations were solicited by BRI during a visit to inventory the project area. Those discussions helped tremendously in understanding the area's history as well as present day realities important to those who live, work and play in the area. At two milestones during concept development, the BRI Team authored and mailed newsletters to approximately 100 residents, businesses and property owners within the IDA. The newsletters provided a summary of work and concepts to date as well as contact information for the Township Zoning Administrator to discuss the project or receive additional information. After refinement of the concepts through SC and community input, an open house was held for the public to see the Streetscape Design Plan and share ideas, thoughts, questions, and concerns with the SC and BRI. In total, there were six SC meetings through the course of the project. Like all DDA meetings, the sessions were open to the public.

Informal Interviews

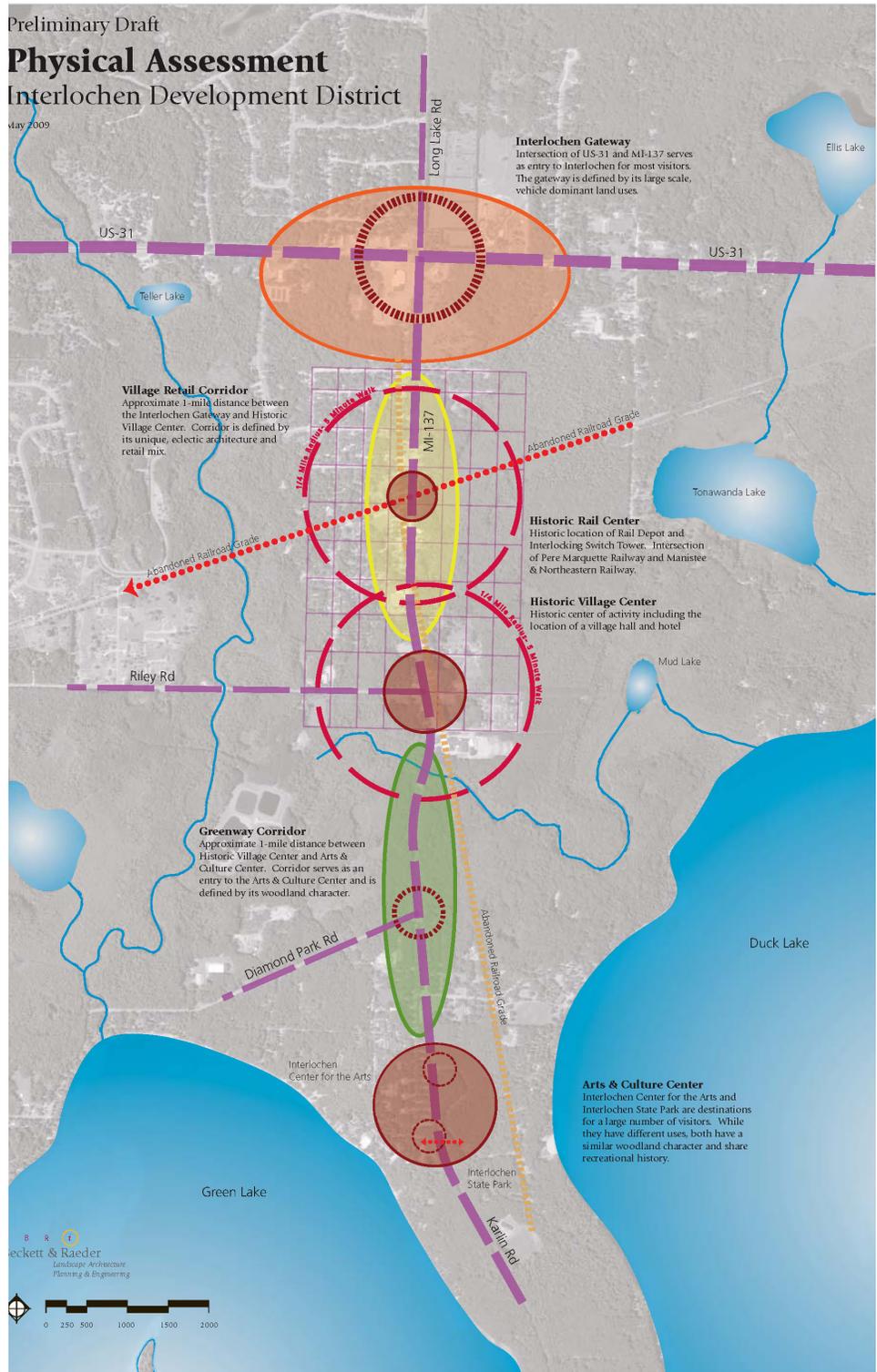
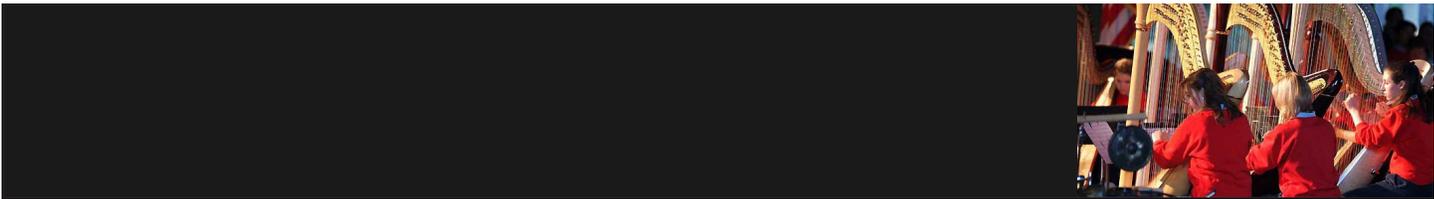
During an early site assessment visit, the BRI Team initiated conversations with a number of residents, businesses and property owners. Members of the community were quite gracious in taking an average of 15-20 minutes to openly discuss their thoughts. The goal of the interviews was to obtain opinions, viewpoints, and feelings relevant to the area, its past and its future that may not have otherwise been expressed in a public context.

Open House

A three-hour community open house hosted by the SC was held in September 2009. During the three hours, community members could come and go as allowed by their schedules. The informal structure allowed SC members and BRI to discuss the project process and resulting designs with small groups averaging 3-4 people. The setting encouraged informal dialogue where people were free to make comments or ask questions specific to their personal interests and concerns.

Preferred Future - Vision for the Village
<i>Arts-related stores and boutiques</i>
<i>Commercial uses which benefit from visitors to the State Park</i>
<i>A civic destination</i>
<i>A year-round commercial core</i>
<i>New businesses located in clustered buildings characterized by small building footprints</i>
<i>Walkable town</i>
<i>Uniquely casual</i>
<i>The elementary school is a central part of the Village activities</i>
<i>Continuation of narrow roads in the Village</i>
<i>Parking: On-street</i>
<i>Village roads paved and public</i>
<i>Pedestrian access along M-137</i>
<i>Lighting along M-137</i>

Source: Interlochen Village Plan - 2007





PHYSICAL OPPORTUNITIES PLAN

The complete Physical Opportunities Plan is described in this section and consists of the Streetscape Design Plan and its resulting recommendations and action items. All have been developed based on the assessment of existing conditions, input from the community and advice from the Interlochen Development Authority Steering Committee. The recommendations are presented by category and where necessary, are further divided into sub-categories. Community priorities identified within the Interlochen Village Area Plan are indicated as such and specifically addressed as part of this Physical Opportunities Plan.

Brand M-137 as “The Avenue of the Arts”

The arts, whether fine, visual, plastic, decorative, applied or performing, are important and character-defining assets within the Interlochen community. There are few places in Michigan, and even in the United States, that match the size and quality of arts community found within the Interlochen area. It is, and should be, a great source of pride and identity. M-137 can become a representation of this identity by being the physical and symbolic entry to the Village of Interlochen and the Interlochen Center for the Arts. As will be described further in subsequent categories, public art in various forms can serve multiple functions in the corridor as identity creation, landmarks, wayfinding, and street furniture elements. One method of funding public art that should be considered by the Interlochen area is a “percentage for the Arts” program. In such programs, a percentage (usually 1%) of public infrastructure project budgets are allocated and held in trust for implementation of public art. In addition to its physical expression within the community, The Avenue of the Arts brand can also be used to further the IDA’s other goals of community organization, promotion, and economic development.



Images from the Philadelphia: Avenue of the Arts web site and event calendar

Key Considerations

- Integrate public art into corridor design as landmarks, wayfinding and street furniture elements.
- Create organizational body for administration of public art projects.
- Consider a “percentage for the Arts” program.
- Develop a holistic “Avenue of the Arts” strategy including physical improvement, community organization, promotion, and economic development components.

Create a Gateway to Interlochen

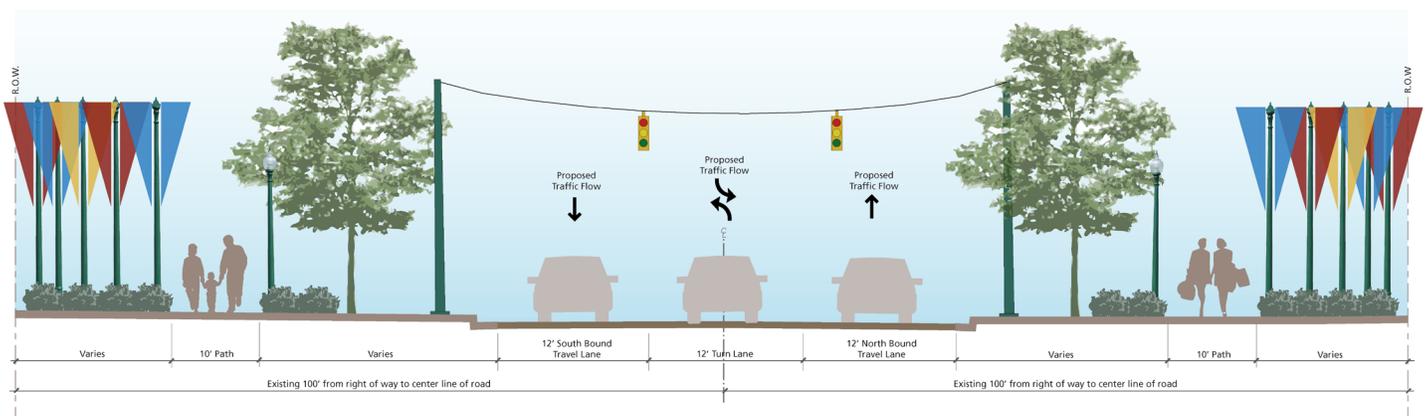
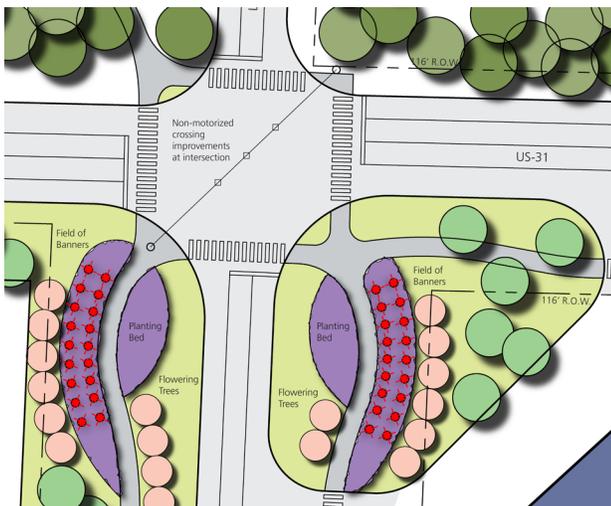
(Community Priority)

The US-31/M-137 intersection is the primary entry point to the Interlochen area for most visitors. Currently, it is a rather nondescript intersection that does not indicate the special character and destinations to the south within Interlochen. In contrast, the streetscape design concept proposes the intersection as a community gateway that reflects the community identity and directs visitors toward Interlochen destinations. Visitors' gateway experiences begin east and west of the "Interlochen Corners" intersection where large, highway scale public art installations signal approach to M-137. Upon arrival to the Corners, fields of banner poles, colorful planting beds and flowering trees on the southeast and southwest corners of the intersection capture attention and intuitively guide visitors south onto M-137. Banners or flags should be designed with a single design gesture to be recognized as a field at highway speeds. The banners or flags could be designed and fabricated by local artists and should be rotated seasonally or on a regular basis to maintain public interest.

The gateway design concept also suggests a roadway aesthetic that will need to be coordinated with future M-137 improvements. Current MDOT-approved improvements to M-137 between US-31 and 1st Street include paved shoulders and adjacent excavated drainage swales. In order to improve overall aesthetic quality and facilitate a gateway experience befitting the community, this study suggests curb and gutter be installed between US-31 and 1st Street. Curb and gutter will minimize the required roadway width and eliminate the need for excavated drainage swales adjacent to the roadway. The concept will also prevent disturbance to several very large oak trees on the west side of M-137. Storm water could be collected in a localized storm sewer system. Existing soils are very sandy and are well suited for infiltration facilities which could be located above or below ground within the public right-of-way.

Key Considerations

- Coordinate with MDOT for improvements within the public right-of-way.
- Incorporate this study's recommendations into M-137 roadway improvement plans.
- Address nonconforming land uses within the public right-of-way.



Conceptual Design - US-31 / M-137 Intersection

Consolidate Future Development into Activity Centers

Strategic steps must be taken to create a purposeful commercial and recreational environment in the Village of Interlochen. This can be accomplished, in part, by recognizing that the Village is an economic machine and its success relies not only on vehicular traffic, but also on pedestrian traffic and the connectivity between points of interest. Today, while cars move steadily through the M-137 corridor, there is simply not enough vibrancy or connectivity to make it work as a center of commerce and community activity. Currently, small-scale, low-density development stretched along a 1-mile corridor between US-31 and 11th Street makes access to multiple destinations difficult. Recharging the Village by building off the connectivity and energy of potential activity centers will help create a better business and social environment.

The vibrancy of the Village will, in part, depend on strengthening the connections among potential activity centers shown on the Streetscape Design Plan. Much like a shopping mall, major anchors at the perimeter help the small shops in the middle. For example, the Historic Rail Center, Historic Village Center, and Arts & Culture Center serve as anchors that create circulation patterns and activities zones between. Creation of these anchors helps the individual elements within the Village function as inter-related components resulting in a higher level of activity and a more successful community environment. Activity centers proposed as part of the Streetscape Design Plan are described below and include:

- Historic Rail Center
- Historic Village Center
- Arts & Culture Center
- Village Business Corridor





Conceptual Design -Historic Rail Village

Historic Rail Center

The Historic Rail Center is proposed at the historic crossing of the Pere Marquette and Manistee & Northeastern railroads. This is also where a railroad depot and intersection tower once stood. Future increased density development is shown on the west side of The Avenue of the Arts and focuses on a central public green. Activity within the Center is supported in part by a future regional trail on the currently abandoned east/west railroad grade. To reflect the Center's historic uses and significance, proposed development where the railroad depot once stood could have architectural detailing reminiscent of period depots. Within the public green, reconstruction of the historic railroad intersection tower serves as a landmark for the Center and a wayfinding element within the larger corridor. The public green is also a stage for community activities and the location of a regional transit system stop. Apart from the historic railroad depot location, new building development in the Center is envisioned as two-story, multi-use with active functions such as retail, dining and entertainment on the first levels.

Less active uses such as office or residential occupy the

upper levels. Proposed development occurs at the public right-of-way and is distinctively "downtown storefront" in character. The Center also includes pedestrian improvements such as sidewalks, lighting, on-street parking, and street furniture to create a pleasant and functional community focused environment.

Key Considerations

- Create long-term strategy to facilitate private development in Center as opportunities arise.
- Create long-term strategy for public infrastructure within Center in order to facilitate private development.
- Adjust current zoning or create overlay district to facilitate desired Center uses and character.
- Develop IDA design guidelines for desired character of future development.



Vision - Mixed Use Development integrated with the non-motorized trail

Historic Village Center

The Historic Village Center is proposed at the intersection of M-137 and Riley Road. This is where the original Village Hall and a hotel once stood. The northeast portion of this intersection is envisioned as a civic complex consisting of a new village hall, library, community center and expanded emergency services facility. Northwest and southeast of the intersection are mixed use developments. All new development should be two-story and present a pedestrian friendly façade at the public right-of-way. The civic complex buildings should have forms and detailing befitting of quality public architecture. Like in the Historic Rail Center, private multi-use developments should be “downtown storefront” in character with active functions such as retail, dining and entertainment on the first levels and less active uses such as office or residential occupying the upper levels. Also like the Historic Rail Center, the Village Center is to include sidewalks, lighting, on-street parking, and street furniture to facilitate a pedestrian friendly environment.

- Create long-term strategy to facilitate private development in Center as opportunities arise.
- Create long-term strategy for public infrastructure within Center in order to facilitate private development.
- Create long-term strategy for development of civic complex directly adjacent to M-137 and 10th Street right-of-ways.
- Adjust current zoning or create overlay district to facilitate desired Center uses and character.
- Develop IDA design guidelines for desired character of future development.

Key Considerations



Conceptual Design - Historic Village Core



Utilization of uniquely designed banners to create place and connect activity centers

Arts & Culture Center

The Arts & Culture Center is well established and consists of the Interlochen State Park and Interlochen Center for the Arts. A unique identity and pedestrian linkages exist within the Center today. However, the Center lacks strong connectivity to the Village and larger Avenue of the Arts corridor. It is the recommendation of this study that non-motorized pathways and public art installations be used to better link the Arts & Culture Center with the rest of the corridor, both in practice and perception. The non-motorized pathways and public art will be described further in subsequent categories.

Key Considerations

- Coordinate with Interlochen Center for the Arts and Michigan Department of Natural Resources to implement pathway and public art recommendations.
- Coordinate with MDOT to implement non-motorized crossings of M-137.

Village Business Corridor

The Village Business Corridor is located between the Historic Rail Center and the US-31 Intersection. As development occurs, uses that are vehicular dominant, less active or otherwise not consistent with Historic Village Center or Historic Rail Center character should be encouraged to locate in this zone. Architecture should maintain the small-scale, eclectic character present in the Village today. Effort should also be taken to maintain a rural and rustic site character as part of new development. Elements currently contributing to that character include large canopy trees, generous site to floor area ratios, gravel parking areas and artisan signage.

Key Considerations

- Adjust current zoning or create overlay district to facilitate desired Corridor uses and character.
- Coordinate with MDOT to minimize size and scope of future driveway improvements.
- Develop design guidelines for desired character of future development.

Create Significant Public Spaces For Events

Public spaces have long played a pivotal role in determining the success of a downtown. According to Fred Kent of the Project for Public Spaces, a non-profit firm dedicated to the study of public places, “we have not even scratched the surface of the impact parks could have on the revitalization of communities.” Urban parks, integrated within a downtown take on many important roles as the center of the public realm. Again, quoting Fred Kent, “a park and its surrounding area is not only a place to understand and relate to nature, but it can also be a place for social and cultural exchange. A park can be alive and teeming with entrepreneurial activities such as markets; physical activities such as children playing or people skating, walking or jogging; or cultural activities, such as art and community events; or for simply socializing with friends”.

Within the Historic Rail Center, the central green increases prominence of the public realm and gives Interlochen a physical expression of its importance to the community. The park is ideal for farmer’s markets, art festivals and other events with a large vendor presence. Of daily importance, the park serves as a community landmark and a destination for users of the future multi-use trail.

Key Considerations

- Provide adequate space and facilities for local and regional events.
- Create multi-purpose spaces.
- Program year-round events.
- Determine revenue-generating opportunities.
- Design spaces for daily use as well as event use.
- Provide lighting and auxiliary power facilities for vendors.
- Provide public restrooms (possibly in the railroad tower).
- Encourage adjacent businesses to activate the public green when not in use for events.

Encourage and Facilitate Sidewalk Activities

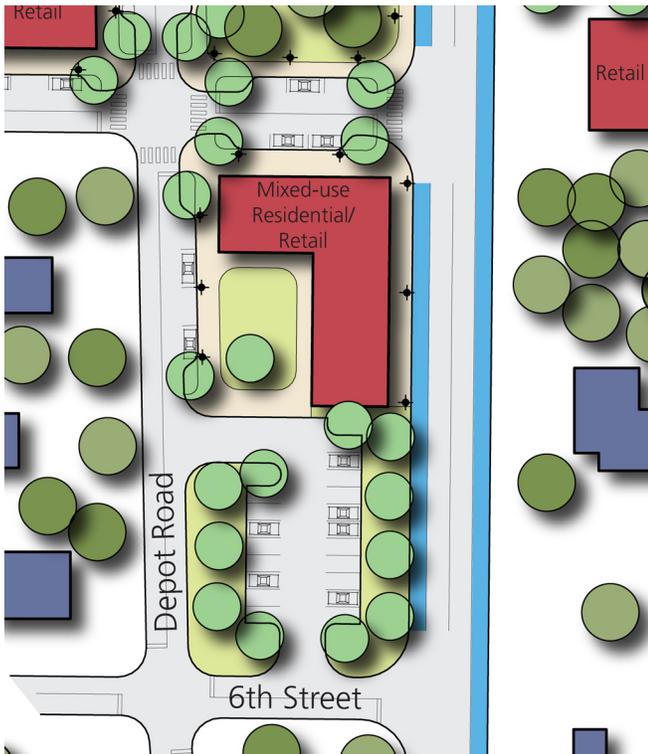
Sidewalk activities are visible evidence of vibrancy and should be encouraged within the Historic Centers. Sidewalk activities could include cafés, seating, merchandise displays, street vendors, or other uses that bring vitality and uniqueness. All are easily implemented without much capital investment. In many cases, it only takes suggesting the possibility to business owners.

By definition, a sidewalk café is an outdoor eating area located on a public sidewalk or park operated by an adjoining eating establishment. This type of open-air eating environment can create a European-style atmosphere and make the streets appear more active resulting in a magnetic appeal to the Historic Centers. In situations where sidewalks may be too narrow for large tables and chairs, even small tables and merchandise displays can help to create a stronger sense of success downtown. Another opportunity is utilizing public spaces when they are not in use for community events. The edges of parks and plazas are ideal locations for outdoor vendors or merchandise displays.

If improperly located, outdoor dining and displays can clutter a sidewalk and inhibit pedestrian traffic flow. While it is beneficial for business district sidewalks to be crowded with people and activities, it is also important to determine appropriate clearance and placement standards. Other important considerations are to determine local regulations for outdoor cafes, feasible locations, and provide design assistance for appropriate furniture types.

Key Considerations

- Encourage merchants to provide outdoor seating and/or merchandise displays.
- Evaluate leasing arrangements and fees.
- Provide furniture material and placement guidelines to merchants. Encourage merchants to coordinate furniture design with the public art board or commission.



Location of off-street parking facilities which serve multiple businesses accessed through the Village street network

Circulation and Parking

Parking is an important component of a successful business district and is often one of the most discussed topics during a master planning process because the adequacy and location of parking can significantly influence the degree of success of a district. However, inappropriate placement and design of parking is also the most frequent source of blight and undesirable character in business districts. The quantity, distribution and character of parking must all be considered for the health of the district. Character of parking facilities is specifically discussed within the section titled Avenue of the Arts Character.

The Streetscape Design Plan responds to these realities by locating parking lots adjacent to Village and Rail Center destinations while minimizing their visual presence from the street. Also in an effort to minimize visual presence, all Center parking lots are accessed from Village streets rather than M-137. This is an access management concept preferred by MDOT and maintains Village control of driveway improvements. Another benefit is visible activity created by users entering through the heart of the Centers to access parking rather than entering and exiting at the edges of the districts. Activity is also made visible by on-street parking within the Centers.

Parking Quantity

Given the inherent nature of the Historic Rail Center and Historic Village Center as centralized nodes of commerce, a parking strategy should be implemented to obtain maximum benefit of available parking. Typical ordinance parking standards result in each land use activity being allocated parking spaces per the zoning ordinance. Relying on this methodology can result in higher demand counts for parking because it treats each visit to a specific business as one parking trip. In reality, a customer to the Centers will likely park once and visit several establishments during their stay. In addition, many of the Centers' land uses will have different peak use days and times. As a result, parking as required by zoning ordinance may not be used to its full potential if specifically allocated to a particular business or land use.

In 2006 the Urban Land Institute (ULI) released its publication on shared parking. Shared parking is a parking strategy where land uses with different parking demand patterns are able to use the same parking space throughout the day. For example, residents living within the Centers will have different parking demands than retail businesses.

As a result, the same parking space can accommodate resident parking in the evening and retail customer parking during the day. The same situation applies to retail businesses, offices, churches, and restaurants, which have different parking demand cycles. The table below enumerates the percentage of parking demand by time of day and land use type.

The Shared Parking Demands Patterns chart illustrates how different uses have different needs through the hours of a day. The same can be true over the days of a week. For example, high demand for office uses occurs during the day on weekdays while high demand for churches and entertainment venues occurs on weekday evenings and weekends. In order to maximize efficiency of available parking, the Interlochen Development Authority should look for adjacent land uses with compatible parking demand cycles and should explore ways to assist both in pooling parking resources.

Key Considerations

- Adjust current zoning or create overlay district to allow and encourage shared use parking.
- Consolidation of individual public and private parking into a collective parking facility.
- Where feasible, allow on-street parking within the Centers.
- Centralize waste and storage facilities in remote and less central areas of a parking lot.
- During the peak seasons and special events require employees to park in remote parking lots elsewhere in the corridor and be shuttled to the Centers.
- During peak seasons and special events use remote parking lots supplemented with shuttle service for customers.

Shared Parking Characteristics					
	<i>Shopping</i>	<i>Fine Dining</i>	<i>Family Dining</i>	<i>Office</i>	<i>Residential</i>
<i>6 a.m.</i>	1%	0%	25%	3%	100%
<i>7 a.m.</i>	5%	0%	50%	30%	90%
<i>8 a.m.</i>	15%	0%	60%	75%	85%
<i>9 a.m.</i>	35%	0%	75%	95%	80%
<i>10 a.m.</i>	65%	15%	85%	100%	75%
<i>11 a.m.</i>	85%	40%	90%	100%	70%
<i>Noon</i>	95%	75%	100%	90%	65%
<i>1 p.m.</i>	100%	75%	90%	90%	70%
<i>2 p.m.</i>	95%	65%	50%	100%	70%
<i>3 p.m.</i>	90%	40%	45%	100%	70%
<i>4 p.m.</i>	90%	50%	45%	90%	75%
<i>5 p.m.</i>	95%	75%	75%	50%	85%
<i>6 p.m.</i>	95%	95%	80%	25%	90%
<i>7 p.m.</i>	95%	100%	80%	10%	97%
<i>8 p.m.</i>	80%	100%	80%	70%	98%
<i>9 p.m.</i>	50%	100%	60%	30%	99%
<i>10 p.m.</i>	30%	95%	55%	10%	100%
<i>11 p.m.</i>	10%	75%	50%	0%	100%
<i>Midnight</i>	0%	25%	25%	0%	100%
<i>Source: Urban Land Institute</i>					



On-street parking increases parking supply; calms traffic; and enhances street walkability

On-Street Parking

As a general rule, parking on as many Historic Village and Rail Center streets as possible provides the most cost effective means for accommodating convenient and desirable parking. Redevelopment of the Center areas should consolidate or eliminate curb cuts so the maximum amount of on-street parking can be introduced. With reconstruction of roadway cross sections during development of the Historic Rail Center, Depot Road, E. Railroad Commons, N. Railroad Street, and S. Railroad Street have possibilities for on-street parallel parking. Riley Road, 9th Street, 10th Street, and 11th Street could all accommodate on-street parking within the Historic Village Center. While some of these streets today may not seem to be prime candidates for people looking for places to park, as redevelopment occurs, there will be more desire for parking within the districts.

Key Consideration

- Provide on-street parking on as many Center streets as possible.
- Develop an access management plan to minimize curb cuts.
- Determine loading zone, emergency access, bus stop, and other curb use requirements.
- Determine optimum lane configurations and parking layout.

Non-motorized Transportation

(Community Priority)

Provision of adequate and well-designed non-motorized facilities is a key component of any successful business district and is particularly important within the Avenue of the Arts corridor. The non-motorized public realm is the conduit that facilitates access to successful goods and services businesses by visitors, residents and ICA students. Non-motorized facilities can take the form of sidewalks, pedestrian cut-throughs, on-street bicycle lanes, off-street pathways, boardwalks, etc. Within the Historic Village and Rail Centers, sidewalks should be readily available and well maintained. Sidewalks and pedestrian cut-throughs need to make businesses and parking easily accessible. At a larger scale, bicycle lanes and pathways should be implemented to provide safe passageway to and from Interlochen State Park, Interlochen Center for the Arts and residential developments north of US-31. Local facilities also intersect with the future multi-use rail trail and provide access to and from other regional destinations.



On-street parking increases parking supply; calms traffic; and enhances street walkability

The Streetscape Design Plan proposes sidewalks be added within the Historic Village and Rail Centers along with other pedestrian streetscape improvements and street reconstruction to accommodate on-street parking. As part of those improvements, crosswalks at all intersections could be highlighted with a special material, i.e., brick, colored concrete, colored asphalt, etc. Sidewalks and pedestrian cut-throughs designed within future buildings directly link off-street parking facilities to destinations within the Centers.

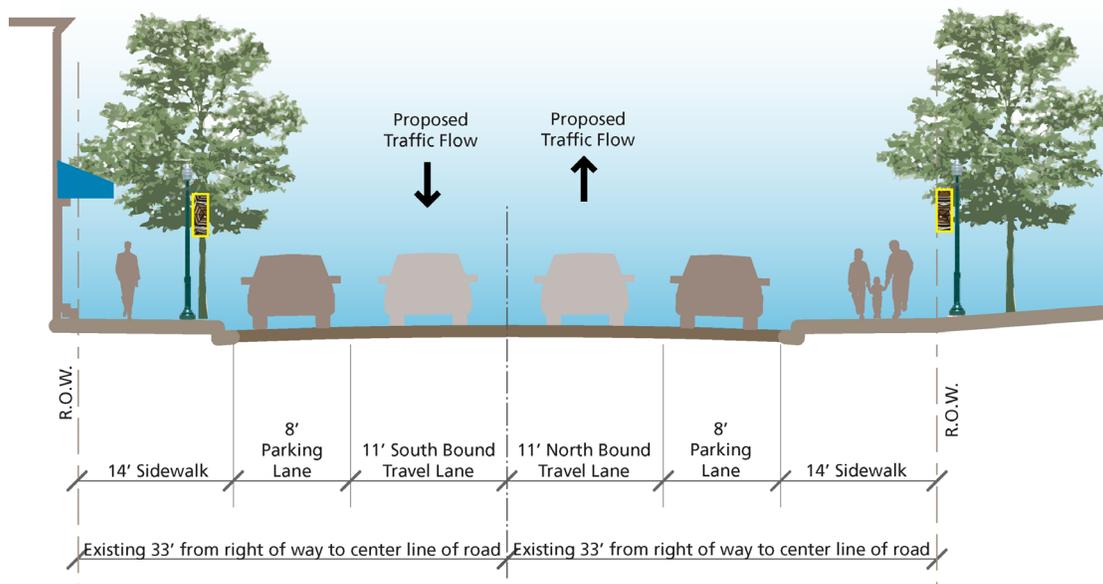
Designated non-motorized lanes and pathways are proposed to stretch the full length of the Avenue of the Arts corridor and provide improved access between the Arts and Culture Center, Historic Village Center, Historic Rail Center, Village Business Corridor, and residential developments north of US-31. From 1st Street to south of Interlochen Community School, non-motorized lanes could occupy the existing M-137 shoulders and include new lane markings, colorized lane surfaces and vibratory warnings. North and south of those boundaries, off-street pathways are proposed to continue non-motorized connections to the Arts and Culture Center and residential developments north of US-31.

Several non-motorized crossings of M-137 are proposed within the Avenue of the Arts corridor. The goal is to create consistent repetition of crossing locations and vocabulary so the crossings are recognized and expected by drivers rather than being isolated occurrences. Each crossing is proposed to have delineation of the surface through pavement marking or special pavements, signage, over-

head lighting, in-pavement user activated lighting, and public sculpture. The public sculpture should be large for easy viewing at highway speeds and could be set within a small plaza or resting area for pathway users. M-137 crossings occur at 1st Street, south of the creek at Interlochen Community School, Diamond Park Drive and Interlochen Center for the Arts entrance. MDOT standardized crossings occur at the US-31/M-137 intersection. Public sculpture is specifically discussed within the section titled Avenue of the Arts Character.

Key Considerations

- Coordinate with Interlochen Center for the Arts to implement non-motorized path south of Interlochen Community School.
- Create public art board or commission to implement crossing sculptures.
- Develop IDA design guidelines for pedestrian connectivity and pedestrian cut-throughs to be incorporated in new development.
- Develop and implement a regular maintenance and replacement program for non-motorized infrastructure. Particularly for colorized non-motorized lanes.
- Be a bicycle friendly destination. Provide bicycle specific signs.
- "Walk Your Bike" policy should be implemented for sidewalks.
- Shared use arrows can be applied to local streets to remind motorists that will be sharing a lane with bicyclists.
- Place bicycle parking at desirable locations throughout the corridor.





Avenue of the Arts Character

(Community Priority)

A common language of art, landscaping, lighting, paving, and street furniture will help create a strong identity for the Avenue of the Arts. It is important to cultivate a family of elements that reflects the corridor’s character and ties its many built elements together with a timeless appeal.

As described previously, art is an important and character defining asset within the Interlochen community and can be used to give the Avenue of the Arts corridor a strong identity. Opportunities for multiple forms of art are available throughout the corridor. Sculpture is proposed to heighten crosswalk recognition by motorists. A field of banners is proposed as part of the Interlochen Gateway at US-31. In addition, public art such as murals, mosaics, smaller sculpture, landforms, light patterns, and other forms may also be incorporated in the public realm as it is shaped by future development.

The community should consider creating a board or commission to administer a public art program. The board could have responsibility for creating a public art master plan, authoring RFP’s for public art, reviewing public art proposals, selecting artists for commissions, maintaining art once installed, and acquiring funding for future art installations. One method of funding public art that should be considered by the Interlochen area is a “percentage for the Arts” program. In such programs, a percentage (usually 1%) of public infrastructure project budgets are allocated and held in trust for implementation of public art.

Key Considerations

- Create board or commission for public art.
- Explore funding mechanisms for public art.



Examples of public art

Landscaping

Street trees are an important ingredient in a streetscape. They provide shade and cool the pavement during hot summer months, and as a vertical element they reduce the perceived width of the street thereby slowing traffic and resulting in a more pleasant pedestrian environment. It is important to select trees that do not block the window displays and signs of downtown stores. Current urban forestry practices suggest that canopy trees have a better chance of survival when they have adequate overhead clearance and a large underground area to support their critical root zone. Small planting holes often inhibit growth and stress the tree. Often there are “green islands” in a downtown where trees can be planted in lawn or landscape areas and have the opportunity to thrive. The public green in the Historic Rail Center and perimeters of Center parking lots are just such locations. When planting street trees, choose a variety of species to avoid noticeable gaps when one dies or is damaged. The current emerald ash borer impact on the ash tree population is a potent reminder that an entire species of street trees can be wiped out in a single growing season.

Other types of landscaping can have a positive impact as well. Any addition of color lets visitors to the district know that it is a well cared for, vibrant place. Encourage business owners to complement their buildings with appropriate flower boxes and pots. Provide design assistance to ensure the use of quality materials and construction. Appropriate materials for window boxes include terra cotta, cedar, painted wood, and wire. Window boxes require plastic liners to extend their life and to maintain moisture in the soil. They should be installed below the sill line of first, second and third story windows. Flower pots of plastic lined terra cotta, ceramic, metal, or wood can be arranged at the entrances to shops. Daily watering is often required in the summer months to keep the plant material healthy. Business owners should use authentic materials whenever possible and avoid the use of artificial flowers and greens.

Key Considerations

- Invest in seasonal plantings downtown.
- Implement IDA design standards that maximize planting bed size. All planting areas should provide adequate drainage and irrigation.

Lighting

A well-lighted corridor will create an environment that feels safe, inviting and comfortable. Existing, overhead pole lights located at intersections along the M-137 corridor positively contribute to the corridor’s rural and rustic character. This treatment should continue at all intersections outside of the Historic Village and Rail Centers. Within the Centers, smaller scale poles and fixtures should be used to create a pedestrian friendly environment. Many “inverted dog dish” style fixtures exist at various locations within the district today. The style positively contributes to the area’s character and should continue to be used for pedestrian fixtures. The pedestrian fixtures should be designed with light sources and pole spacing that provide the minimum required light level with the maximum possible uniformity.

Opportunities should also be explored for highlighting of special locations. For example, multiple head fixtures could be used at significant intersections and public locations. Uplighting or pin-lighting could be used to highlight elements such as the Interlochen Gateway banners, rail intersection tower, crossing sculptures, public art and other special items.

Key Considerations

- Install appropriately scaled pedestrian light fixtures along with other streetscape improvements.
- Identify significant elements for special lighting.
- Explore fixture photometrics and layout to provide maximum possible light level uniformity.



Examples of unique street furniture

Street Furniture

It is a recommendation of this Streetscape Design Plan that the street furniture reflect Interlochen’s rich arts community and heritage. Benches, trash receptacles, bicycle racks, planters and other street furniture elements could be designed and fabricated by local artists within guidelines of a public arts board or commission. Each piece of street furniture could be unique such that no two benches, planters or other elements within the corridor are the same. Where appropriate, the design of street furniture could reflect the personality of adjoining businesses or land uses. In addition to adding a fun and whimsical aesthetic to the corridor, the unique pieces of street furniture will also serve as landmarks and wayfinding aides.

Wherever possible, moveable seating should be used to give people options to move chairs to catch moving sun patterns or nearby activities. If the area is well attended, theft can be minimal—though moveable chairs often need to be stacked and chained at night.

In addition to being an efficient security measure, bicycle racks eliminate the visual clutter caused by haphazard bicycle parking. Bike racks will reinforce the image of the Avenue of the Arts as a recreation and special event corridor in addition to a place to live and work.

Key Considerations

- Include street furniture as a primary goal of a public art board or commission.
- Install benches, moveable seating, trash receptacles,

planters, and bicycle racks in key locations.

Signage

The Avenue of the Arts corridor would benefit from a unified family of wayfinding and directional signage. The signage could identify and direct users to “districts” such as the Historic Rail Center, Historic Village Center and Arts & Culture Center. Within those districts, a secondary set of related signage could guide users to public parking, public buildings, parks, non-motorized facilities, and other specific destinations. Another set of signs within the same family could be used on the non-motorized pathways to provide the same wayfinding and directional information provided to motorists. The signs should have consistent, identifiable character and be easily read by the intended motorist, bicyclist or pedestrian audience.

Public wayfinding and directional signage should be the primary and visually dominant signage within the corridor. Individual or collections of private business signs should not compete with the public corridor signage. To prevent visual competition and to maintain the rural and rustic character of the corridor, private business signage outside of the Historic Village and Rail Centers should be limited to façade mounted signage and monument style, ground mounted site signage. Façade mounted signs for each business should not project more than 1’ from the building and have a maximum size of 1 sf per linear foot of ground floor frontage up to a maximum of 40 sf. In other words, a business occupying 20 linear feet of ground floor frontage within a larger building would be allowed 20 sf of façade mounted signage. Each building should also be allowed monument style site signage of 1 sf per linear foot of ground floor building frontage up to a maximum of 25 sf. In other words, a building with 20 linear feet of building frontage would be allowed 20 sf of monument style site signage. The square footage may be used in a single sign or a combination of multiple signs. If multiple businesses are present within a single building, the allowable square footage for site signage is to be shared among the businesses. The Township should review placement of site signage to ensure proposed locations do not compete with corridor wayfinding or directional signage and do not block turning movement sightlines.

Within the Historic Village and Rail Centers, business signage should be wall and projecting façade signs. Wall signs should have a maximum size of 15% of the first floor façade area occupied by a business or 20 sf, whichever is larger. Projecting signs should have a maximum sign area of 12 sf, maximum projection of 4’ and have a minimum



Simple but informative projecting signs

clearance of 10' above sidewalk.

At the US-31 and M-137 intersection, where businesses are more vehicular oriented, signage should be low-profile, monument-type signs incorporating quality, natural materials. Monument signs should not exceed 5' in height and the width should be in proportion with the business or development it serves. The design should incorporate architectural elements and articulation to provide visual interest.

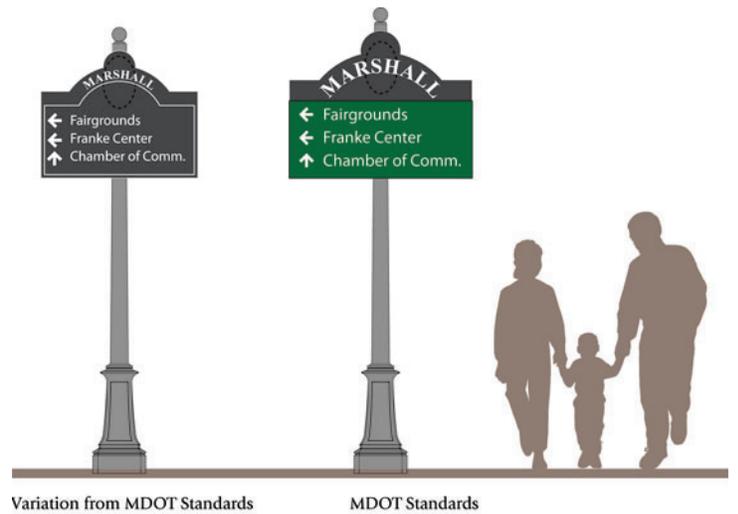
General character and lighting of signage should reinforce the rustic and rural nature of the corridor. Signs should be artisan crafted rather than standard, pre-fabricated units. Refer to photographs for desirable signage examples. Lighting for signs should be external pin lights aimed and shielded to conceal the light source. Internally lighted signs should not be used.

Key Considerations

- Conduct a wayfinding and directional signage study and implementation plan.
- Adjust current business signage ordinance to incorporate Streetscape Design Plan recommendations.
- Develop IDA design guidelines to describe desired signage character.

Paving

Special paving at significant locations can add emphasis and provide visual relief from expansive areas of concrete and asphalt. Concrete walks are recommended as the predominant surface in the pedestrian "through-zone"—the area of a sidewalk intended for pedestrian travel entirely free of permanent and temporary objects. Special paving should be considered as "accent" material to enrich the pedestrian experience while keeping construction costs reasonable. One accent location could include the "lawn extension" areas within the Historic Village and Rail



Wayfinding, directional and regulatory signage system proposed for Marshall, Michigan

Centers. This is the area that would normally be lawn between a sidewalk and curb but it is better paved in higher traffic locations. Other accent locations could include pedestrian bump-outs at intersections, pedestrian crosswalks and significant public spaces such as the area surrounding the rail intersection tower.

Key Considerations

- Create use and material guidelines for special pavements as part of IDA design guidelines.

Street Corner Treatments and Accessibility

Pedestrian activities are often concentrated on street corners at main intersections. These are important areas where people tend to converge, either to talk with others or wait for street crossing opportunities. Some proposed street corners within the Historic Village and Rail Centers have extended sidewalks, referred to as bump-outs, to facilitate shorter pedestrian crossing distances. When replacing or installing new curbs it is preferable to incorporate bump-outs and remember that the tighter the curb radii, the shorter the crossing distance resulting in a better pedestrian environment. Using the tightest, or smallest curb radii possible for the circumstance also require vehicles to slow down as they turn the corner.

All new improvement projects including sidewalks, ramps, landings, pedestrian crossing signals, and pedestrian markings must meet the American with Disabilities Act (ADA) accessibility standards. The ADA is a federal law that protects the rights of persons with disabilities in an effort to assure that they have equal access to public services.

Tactile warnings and textures on ramps for persons with disabilities are also becoming standard practice in corner treatment designs.

Key Considerations

- Incorporate proposed bump-outs and tighter curb radii on new street projects.
- Evaluate the downtown for compliance with ADA accessibility standards.

Parking Lots

Though parking lots are often at the backside of businesses, they also foster a strong first impression and provide back entrance access to shops. Private parking lots within the corridor should include aesthetically appealing lighting fixtures, trash and grease bin receptacle shielding, and basic clarity of layout with clear and unified design elements. A parking lot is a difficult environment for growing trees but properly planted they can offset the heat, glare, and often negative visual impact of these large expanses of paving. Choose plant materials that can withstand harsh urban conditions and de-icing materials, and wherever possible group trees in large planting beds at end islands and in perimeter planting strips.

The IDA should seek to apply similar design treatments to the perimeter of all public parking lots within the Historic Village and Rail Centers. This will improve the appearance of the lots and give first time users visual cues as they seek out parking within the Centers. Levels of treatment may vary, and can include masonry screen walls, decorative fencing, landscape treatments, and public parking signs. Defined entrances are also important to clarify circulation patterns and make the parking lots more user-friendly. The IDA may consider installing new directory signs on pedestrian routes so customers can see a visual display of downtown businesses.

Key Considerations

- Carefully place and shield trash dumpsters and grease bins.



Main Street streetscape in Dexter, Michigan: an example of a coordinated maintenance program.

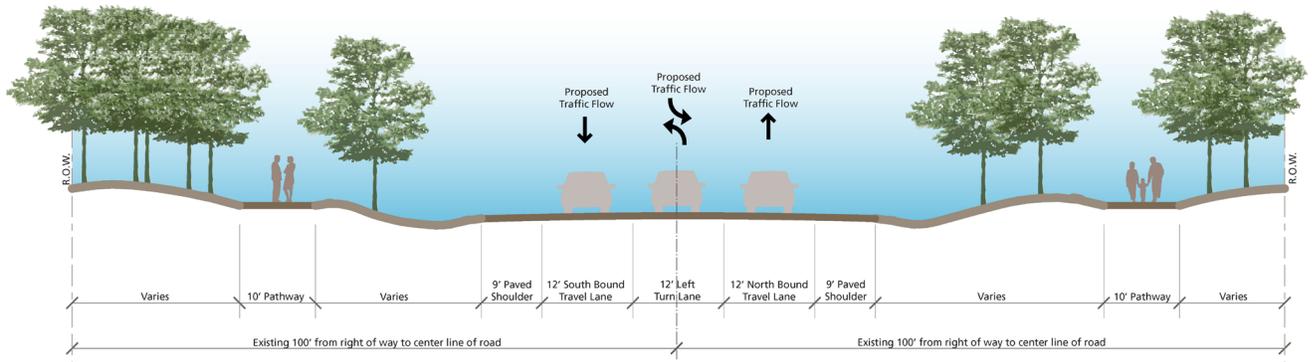
- Establish defined entrances and clarify circulation in all parking lots.
- Incorporate planting and screening treatments in all new parking lot designs.
- Encourage private owners to enhance planting and screening of existing parking lots.
- Apply similar design treatments to the perimeter of all public lots.
- Incorporate parking lot landscaping and screening in IDA design guidelines.

Maintenance

Thoughtful maintenance of the district not only protects investments made in improvements but also presents the Avenue of the Arts at its best. In some ways, the corridor gives two initial impressions. The first is seen through the windows of a passing vehicle. The eclectic architecture, large trees and intimate scale lend it an immediate charm. The second is found while walking the streets and parking lots. This image, though still retaining some of the charm of the initial impression, also reveals the need for maintenance and for more careful attention to detail.

Key Considerations

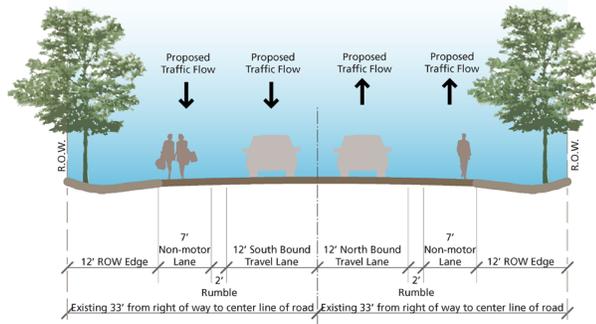
- Develop and implement a regular pavement maintenance and replacement program. Particularly for colorized non-motorized lanes.
- Develop and implement a regular maintenance



Section A Avenue of the Arts (M-137) within Interlochen Gateway

Scale: 1/8" = 1'-0"

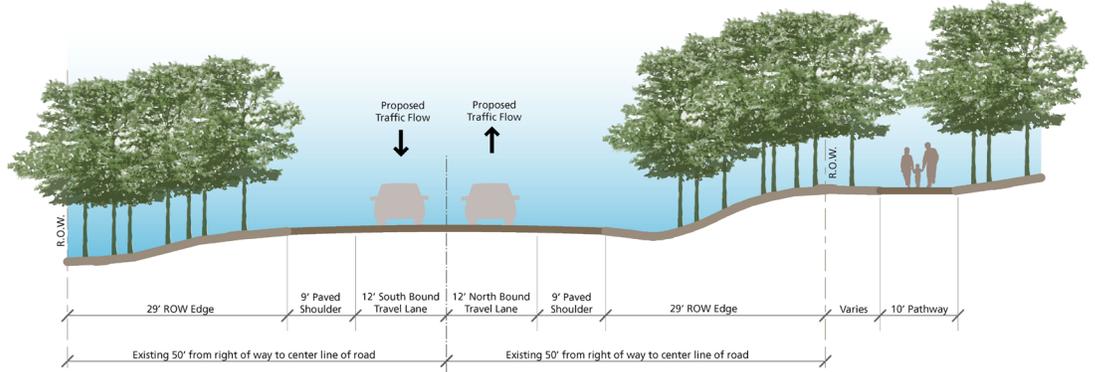
One lane in each direction with left turn lane, non-motorized pathways, and landscape enhancements



Section B Avenue of the Arts (M-137) within Village Business Corridor

Scale: 1/8" = 1'-0"

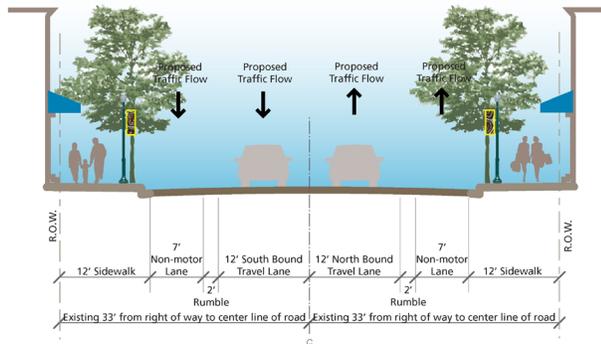
One lane in each direction with non-motorized lanes at shoulders



Section C Avenue of the Arts (M-137) within Greenway Corridor

Scale: 1/8" = 1'-0"

One lane in each direction with non-motorized pathway outside ROW



Section D Avenue of the Arts (M-137) within Historic Village Center

Scale: 1/8" = 1'-0"

One lane in each direction with non-motorized pathway



IMPLEMENTATION

Implementation of the Streetscape Design Plan

In order to assist the Interlochen Development Authority prioritize first steps in implementation of the Streetscape Design Plan, a series of projects have been identified in three categories. Planning projects amend current community planning documents to incorporate improvements proposed in the Streetscape Design Plan or suggest further detailed study of individual master plan components. Acquisition projects identify land required for future public improvements. Construction projects follow through on the master plan's intent in order to realize the community's vision.

Planning Projects

1. Coordinate with 5-year Parks and Recreation Plan *Plan Amendment Cost: (in progress)*

Coordinate the Interlochen Development District Conceptual Enhancement Plan with the community 5-year parks and recreation plan amendment (in progress) to include recreation related land acquisition and development projects described in the Streetscape Design Plan. Acquisition projects that should be included in the revised recreation plan are the parcels required to implement the central Public Green within the Historic Rail Center. Improvement projects that should be included are the Historic Rail Center Public Green, designated non-motorized lanes on M-137 and non-motorized pathways north and south of the Village. Including these acquisition and development projects in the 5-year recreation plan, makes them eligible to be considered for grant sources such as Michigan Natural Resources Trust Fund, Land and Water Conservation Fund, Recreation Improvement Fund, and Recreational Trails Program.

2. Review DDA Development Plan and Tax Increment Financing Plan

Plan Amendment Cost: <\$4,500

Review the DDA Development Plan and Tax Increment Financing Plan to determine if amendments to those plans are necessary and to determine if the projects outlined in the Conceptual Enhancement Plan are consistent with the projects outlined in the DDA Development Plan and Tax Increment Financing Plan.

3. Create Avenue of the Arts Overlay Zoning District Ordinance

Preparation Cost: <\$5,000

Create a new Avenue of the Arts Overlay Zoning District that would allow the desired uses and characters inside and outside the Historic Village and Rail Centers. The Village may also want to consider two overlay districts. The first for the Historic Village and Rail Centers. A second for the Village Business Corridor and other areas outside the Centers. Within the Centers, provisions should be added to allow zero-lot-line development, mixed-uses, live/work units, and residential dwellings in the existing zoning districts. When implementing the overlay district, consideration should also be taken to encourage highly active uses at street level and less active uses on upper levels.

The majority of potential redevelopment parcels are zoned VC (Village Commercial). Permitted or Special Land Use Approval uses within VC include automobile service station, automobile washing facility, veterinary clinic, dry cleaning distribution center and transportation depot. Some of these uses would not advance the Avenue of the Arts vitality or character. In addition, the Planned Unit Development provisions of the zoning ordinance (Article 11) requires eligible PUD sites to have a minimum acreage of five (5) acres which severely limits the introduction of innovative, mixed-use and higher density development. The adoption of an overlay district would provide a framework for a bundle of land uses needed to advance the Streetscape Design Plan and contain needed regulatory provisions to ensure appropriate public realm and private development design.

Avenue of the Arts Zoning Overlay Components
<i>Utilization of PUD provisions regardless of acreage</i>
<i>Allowance for mixed uses within same building</i>
<i>Allowance for adjacent multiple land uses</i>
<i>Requirement for street-level commercial</i>
<i>Allowance for shared parking</i>
<i>Density bonus for upper story residential</i>

4. Develop Avenue of the Arts Design Guidelines

Guideline Study Cost: <\$25,000

Create design guidelines to illustrate desired Avenue of the Arts development character. The guidelines should specifically address the Historic Village and Rail Centers as well as the Village Business Corridor and other areas outside the Centers. Recommendations should be given for development setbacks and site to floor area ratios; architectural forms, massing, materials and transparency; pedestrian connections and cut-throughs; parking facility massing, materials, landscaping and screening; sidewalk dimensions and materials; signage dimensions, materials and lighting; encouragement of and guidelines for outdoor activities such as dining and merchandising; seasonal displays and other design elements integral to Avenue of the Arts character.

5. Prepare Wayfinding Study

Guideline Study Cost: <\$20,000

Prepare an Avenue of the Arts corridor wayfinding and directional signage program. Signage recommendations should address large-scale wayfinding and identity such as location of the Historic Village Center, Historic Rail Center and Arts & Culture center within the larger corridor.

Within those districts, a secondary set of related signage should guide users to public parking, public buildings, parks, non-motorized facilities, and other specific destinations. Another set of signs within the same family should be used on the non-motorized pathways to provide the same wayfinding and directional information provided to motorists. Signs should have consistent, identifiable character and be easily read by the intended motorist, bicyclist or pedestrian audience.

Acquisition Projects

1. Historic Village Center

The Streetscape Design Plan proposes a concentrated center of development referred to as the Historic Village Center at the intersection of M-137 and Riley Road. A critical component of the concept is to create a walkable district by encouraging development to meet the public right-of-way. A future community complex at the north-east corner of the intersection should also be developed directly adjacent to the M-137 public right-of-way. Parcels fronting M-137 in that block would be required to construct the expanded Township facility.

Projects and Programs Summary			
Planning	1	Coordinate with 5-Year Recreation Plan	In Progress
	2	Review DDA Development and TIF Plan	\$4,500
	3	Create Avenue of the Arts Overlay District	\$5,000
	4	Develop Avenue of the Arts Design Guidelines	\$25,000
	5	Prepare Wayfinding Study	\$20,000
Acq.	1	Historic Village Center - Acquisition	N.A.
	2	Historic Rail Center - Acquisition	N.A.
Construction	1	M-137 Reconstruction at US-31 Gateway	\$650,000
	2	US-31 / M-137 Intersection	\$700,000
	3	Designated Non-Motorized Lanes on M-137	\$300,000
	4	Interlochen / Old Village Plat Street Reconstruction	\$2,405,000
	5	M-137 Corridor Improvements	\$1,250,000
	6	Pedestrian Bridge	\$150,000
	7	Wayfinding Implementation	\$150,000
	8	Water System Extension and Water Tower	\$665,000
	9	Sewer System Extension	\$1,770,000
	10	Village of Interlochen Pathway Network	\$450,000
	11	Village of Interlochen Street Reconstruction	\$1,565,000
	12	Village of Interlochen Street Paving	\$1,844,000

2. Historic Rail Center

Like the Historic Village Center, the Historic Rail Center is a concentrated center of development intended to create a walkable commercial district with a significant public open space. In order to develop the framework for the Center, coordination of several properties will be required to re-align a portion of South Railroad Street outside its current right-of-way and create the public green between M-137 and East Railroad Commons.

Construction Projects

1. M-137 Reconstruction at US-31 Gateway

Conceptual Construction Cost: \$650,000

Reconstruction of 1,320 feet of M-137 from the intersection of US-31 south with the installation of curb and gutter, storm sewer, median, lighting and landscaping.

2. US-31 / M-137 Intersection

Conceptual Construction Cost: \$700,000

Install pedestrian safety features at intersection along with sidewalks, landscaping, and lighting.

3. Designated Non-motorized Lanes on M-137

Conceptual Construction Cost: \$250,000-\$300,000

Work with MDOT to convert existing M-137 shoulder lane to designated non-motorized lanes between 1st Street and the bridge south of Interlochen Community School. Install vibratory warnings, pavement markings and non-motorized lane colorization. Conceptual construction costs assume all improvements can be made after cleaning of existing pavement.

4. Interlochen / Old Village Plat Street

Reconstruction

Conceptual Construction Cost: \$2,405,000

Reconstruction of M-137 through the Village of Interlochen from 1st Street to Riley Road. Reconstruction includes installation of storm sewers, curb and gutter, 10' wide sidewalks, pavement resurfacing, lighting, and site restoration.

5. M-137 Corridor Improvements

Conceptual Construction Cost: \$1,250,000

Resurfacing, installation of a 10' wide sidewalk, corridor lighting, storm water improvements, and site restoration along 4,975 feet of M-137 from Riley Road south to the State Park entrance.

6. Pedestrian Bridge

Conceptual Construction Cost: \$150,000

Construction of a pedestrian bridge over Betie Creek which would connect other non-motorized / pedestrian trails.

7. Wayfinding Implementation

Conceptual Construction Cost: \$75,000 - \$150,000

Implement the Avenue of the Arts corridor wayfinding and directional signage program.

8. Water System Extension and Water Tower

Conceptual Construction Cost: \$665,000

Construction of water main extending from US-31 to the Village of Interlochen including the construction of a water tower.

9. Sewer System Extension

Conceptual Construction Cost: \$1,770,000

Construction of sewer main extending from US-31 to the Village of Interlochen including the construction of a treatment facility.

10. Village of Interlochen Pathway Network

Conceptual Construction Cost: \$450,000

Construction of two miles of pedestrian pathways in the Village of Interlochen with the construction of several broadwalks through wetlands.

11. Village of Interlochen Street Reconstruction

Conceptual Construction Cost: \$1,565,000

Installation of curb/gutter, storm sewer and sidewalks along 1.6 miles of streets in the Old Village Plat.

12. Village of Interlochen Street Paving

Conceptual Construction Cost: \$1,844,000

Pavement of 1.4 miles of unpaved streets with curb/gutter, storm sewer, pavement, and resurfacing.